

MEMO

TO: Matthew Kelling - Development Manager
FROM: Arben Durmishaj Sr. Aviation Planner
SUBJECT: 5 Capri Road, the proposed development
DATE: November 04, 2022

INTRO

Tenblock contacted WSP Canada Inc. to review the new proposed development at 5 Capri Road, based on GTAA - AZR height restriction of the outer surface.

BACKGROUND

Based on GTAA AZR¹, no person shall erect or construct, on land to which these Regulations apply, a building, structure or object, or an addition to an existing building, structure or object, the highest point of which will exceed in elevation at the location of that point of the outer surface.

The AZR Outer surface elevation is **219.46m ASL**, based on the CVGD:1978.

The subject site, 5 Capri Road, is located within the GTAA-AZR, and under the Outer Surface as shown in figure 1.

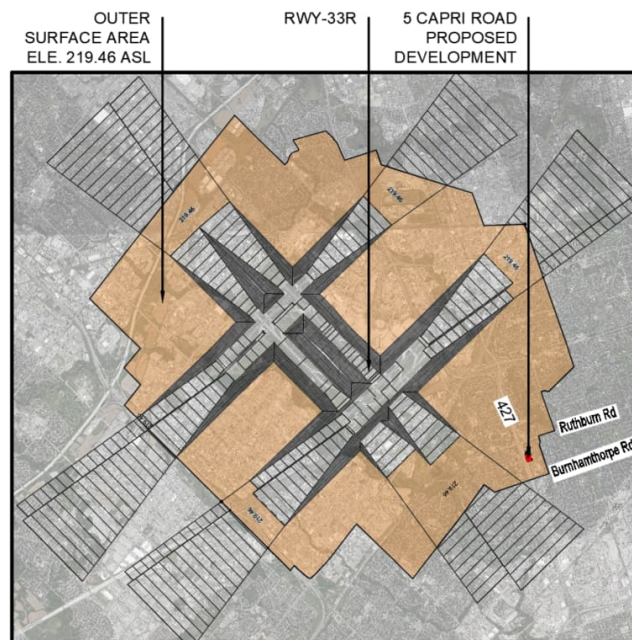


Figure 1 – GTAA-AZR – 5 Capri Road proposed location

In March 2021, Tenblock submitted a development application to city of Toronto, for three (3) residential buildings at 5 Capri Road, the proposed building height details are shown on table 1.

¹ <https://laws-lois.justice.gc.ca/PDF/SOR-99-123.pdf>

	BUILDING A	BUILDING B	BUILDING C
FFE (m) ASL	145.9	145.9	145.9
Height (m)	91.7	127.4	103.5
ASL (m)	237.6	273.3	249.4
Distance above AZR Outer Surface (m)	18.14	53.84	29.94

Table -1 Building height details

As per GTAA comment letter² all the buildings violate the AZR outer surface height limits, by **18.14/53.84/29.94m**. Subsequently the development proposal has been revised so the building height does not violate the AZR Outer Surface, and new drawings were submitted to WSP for review.

ANALYSIS

The proposed development at 5 Capri Road, is located under the AZR outer surface, and the closest point of the building B to the Approach Surface of RWY 33R is 556m away, as shown in figure 2.

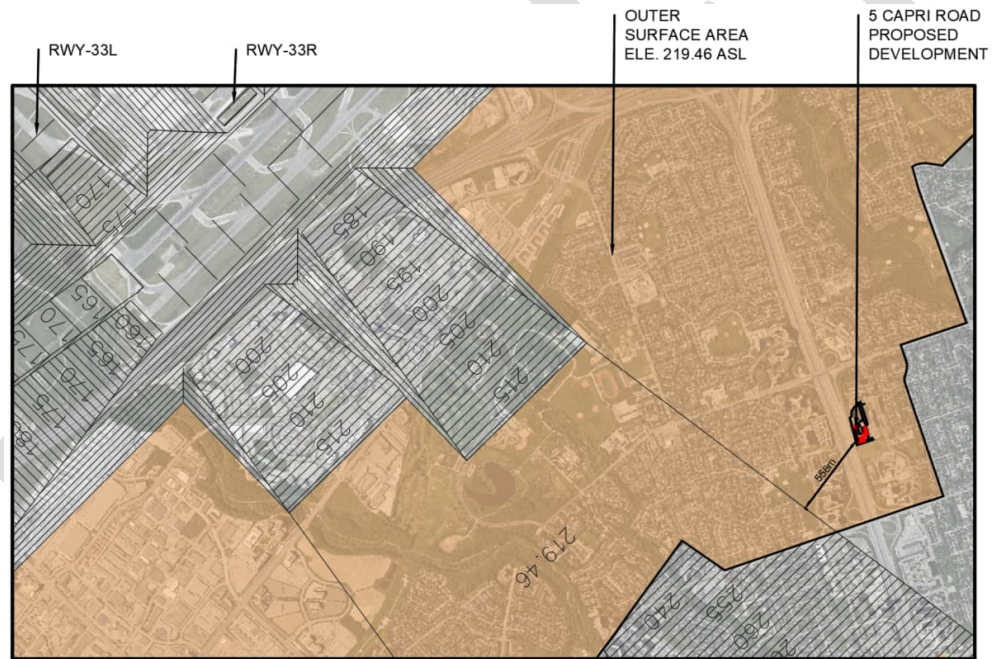


Figure 2 – 5 Capri Road proposed location

The outer surface for the GTAA is an imaginary plane established at constant elevation of **45 m** above the aerodrome reference point (airport reference point in the AZR [description](#)). The Airport reference point is published in the AZR description as **174.46m** ASL.

² 2021-07-23 - Comments from GTAA - 1st Submission - 5 Capri

GEODETIC DATUM

As indicated by GTAA, the geodetic datum (vertical) of the AZR is CGVD28:1978. However, the elevation of the proposed development at 5 Capri Road is based upon CGVD28:Pre1978. At this location, the conversion factor is approximately 0.118m; in other words, 0.118m should be added to CGVD28:1978 to equate with CGVD28:Pre1978. Therefore, the AZR limit at this location, in CGVD28:pre1978, is 219.58 m ASL.

MAXIMUM ELEVATIONS

The revised development application still has three residential buildings, but the building height has been reduced to comply with GTAA AZR. Table 2 shows the proposed tower height details and compliance.

	BUILDING A	BUILDING B	BUILDING C
FFE (m) ASL	145.8	145.8	145.8
Height (m)	73.5	73.5	73.5
ASL ³ (m)	219.0	219.0	219.0
Distance below AZR Outer Surface (m)	0.58	0.58	0.58

Based on the drawings provided all the buildings have a flat roof, and no other structure is shown that is above the MPH Roof.

The elevation drawing, figure 3, shows the elevation of the top portion of the building B, in relation to the AZR Outer surface, as per the drawings MPH roof is **0.58m** below the AZR Outer Surface.

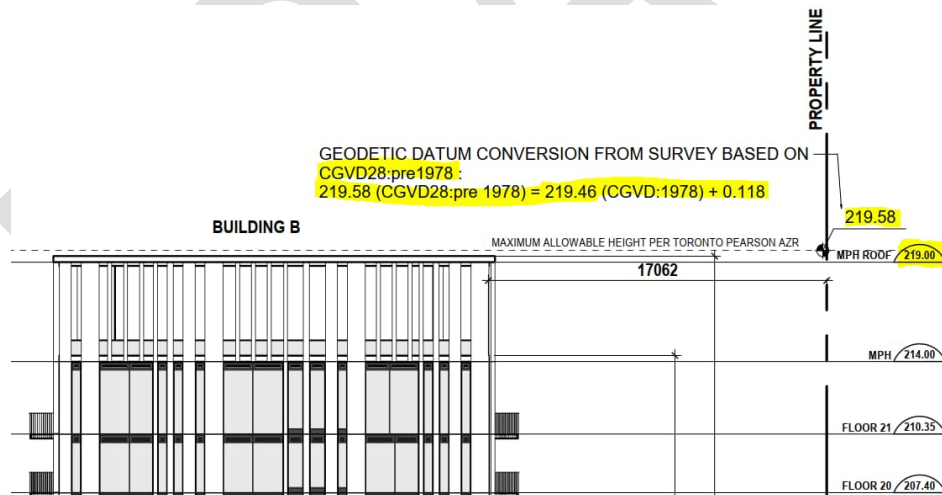


Figure 2 – Building B Elevation⁴

CONSTRUCTABILITY

³ ASL elevation as per CGVD28: pre 1978

⁴ Figure is extracted from drawing "2022-10-28-5 Capri Road-Elevations and Sections.pdf"



Constructability and crane analysis were not part of this scope, and will be dealt on later stage, by Tenblock.

Typically, GTAA and NAV Canada requires, a submission crane form, at least 10 business days prior to the start of the construction.

CONCLUSION

Based on the foregoing, WSP has identified that the proposed development at 5 Capri Road, as presented to WSP by Tenblock and documented within this letter and enclosure, the rooftop of all three buildings is below the AZR Outer Surface by **0.58m**.

WSP would like to remind the proponents for this project that it is the Applicant's (Tenblock) responsibility to ensure compliance with AZR is maintained at all times. Should ownership of the property change, the responsibility for compliance would then shift to the new owner. Any deviation from what is proposed, the GTAA must be informed.

This letter and stated observations should not be construed as a letter of support or rejection for the development proposal that has been assessed.

Tenblock, has received a letter of no Objection from NAV Canada⁵ dated December 13 2021, (file #21-2011).

The developer should submit the following application:

- Aeronautical Assessment Form to Transport Canada⁶

Should you have any questions or concerns, please do not hesitate to contact the undersigned.

Sincerely,

Arben Durmishaj - Senior Aviation Planner

ACRONYMS

AIA	Aeronautical impact assessment
ASL	Above sea level
AZR	Aerodrome Zoning Regulations
CGVD	Canadian Geodetic Vertical Datum
ELE	Elevation
FFE	Finished Floor Elevation
GTAA	Greater Toronto Airport Authority
MPH	Mechanical Penthouse
OLS	Obstacle Limitation Surfaces
TC	Transport Canada

⁵ 21-2011 Letter to proponent

⁶ https://wwwapps.tc.gc.ca/Corp-Serv-Gen/5/forms-formulaires/download/26-0427_BO_PX